

City of Nanaimo

REPORT TO COUNCIL

DATE OF MEETING: 2013-DEC-16

AUTHORED BY: GARY NOBLE, DEVELOPMENT APPROVAL PLANNER,
PLANNING & DESIGN SECTION

RE: DEVELOPMENT PERMIT NO. DP755 - 3217 HAMMOND BAY ROAD

STAFF RECOMMENDATION:

That Council issue Development Permit No. DP755 at 3217 HAMMOND BAY ROAD subject to an access easement to the adjacent property, and with support for the following variances:

- To increase the maximum building heights
 - Building 1 to 10.15m
 - Building 2 to 9.70m
 - Building 3 to 10.45m
- To reduce the Minimum Landscape Treatment Level 2d to a 0.2m width.

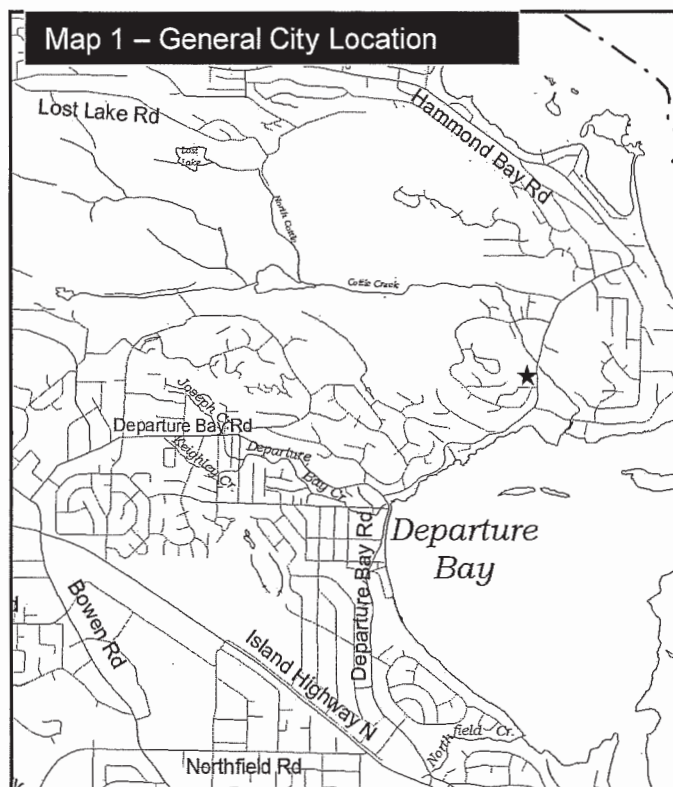
PURPOSE:

The purpose of this report is to seek Council authorization to issue a development permit for a 10 unit, cluster housing development.

BACKGROUND:

A development permit application was received from STRAIGHT STREET DESIGN (Mr. Kenneth Brault), on behalf of Sikander and Sharanjit Saroya, to permit the construction of a 10 unit (1 existing single family dwelling, and 9 new units) cluster housing development.

Staff and the Design Advisory Panel (DAP) support the application, including the proposed variances for each of the clusters.



Council
 Committee.....
 Open Meeting
 In-Camera Meeting
Meeting Date: 2013-DEC-16

Site/Landscape Design

1. Site Access

Site access design and location have been constrained by a number of factors:

- Siting of the existing SFD or Unit #10.
- Need to provide a future access to the neighbouring property to the north (3219 Hammond Bay Road).
- Significant grade change between Hammond Bay Road and the existing SFD.
- Width of the panhandle portion of the site.

Site access grades have been approved for fire truck access.

The site access and parking court design allows for 14 parking spaces for the 9 units. There is provision for 2 visitor parking spaces, and Unit #10 also has 2 parking spaces. The onsite parking exceeds the required onsite parking spaces by 4.

The site access also accommodates a future access easement to the adjacent property (3219 Hammond Bay Rd).

Site Development Strategies

An infill development within an established neighbourhood presents challenges that are addressed through the following strategies:

1. Mitigation Plan for Trees on 3219 Hammond Bay Road, which Abuts the Onsite Access Road for the Subject Property

The City's Urban Forester and the developer's certified arborist have completed a mitigation plan to protect the significant Arbutus tree and several Douglas fir trees. The location of the impacted trees is identified in Schedule C1. The mitigation plan is within the Prohibitions and Guidelines of the City of Nanaimo "Management and Protection of Trees BYLAW 2013 NO. 7126".

An air tool is used, instead of an excavator, for trenching within the root zone of the significant Arbutus tree and Douglas fir trees. This procedure minimizes damage to the tree roots which can then be pruned as required or moved aside to facilitate the placement of utility pipes. This procedure will be monitored by the certified onsite arborist.

The certified arborist has established a contract agreement with the developer to provide onsite supervision during site development. The City will secure site supervision as a condition of the building permit.

2. Access Issue for 3219 Hammond Bay Road

The neighbouring property to the north (3219 Hammond Bay Road) has a long standing gravel driveway which goes across the frontage of the subject property in order to access Hammond Bay Road (see Schedule I). The preference of the current owner of 3219 Hammond Bay Road, is to retain this existing access.

The subject property development requires full frontage works and services. The construction of these works and services would necessitate the removal of the gravel driveway to 3219 Hammond Bay Road.

In response to the request to retain the existing access to 3219 Hammond Bay Road, Staff have identified an interim works and services standard. The interim standard would include:

- full works and services from the south property line of the subject property to the north side of the subject property curb drop; and
- the re-grading of the driveway, in order to function with the interim works and services design (see Schedule J).

The full frontage works and services for the subject property will be secured as a condition of the building permit.

An access on the subject property to 3219 Hammond Bay Road is to be constructed as part of the proposed development, in order that the gravel driveway can be removed at a later date. An access easement is to be registered on the subject property to secure this access through the subject property.

Landscape Plan

The landscape plan uses four different strategies in order to provide privacy for this infill project and to retain the character of the site.

- A green wall fence system is used as a screen along the north property edge where the access road precludes the required Minimum Landscape Treatment Level 2d.
- The property edges, which abut residential properties and need screening, are planted with a western red cedar hedge and random deciduous and coniferous tree plantings.
- The ground plane is planted with a typical urban forest understory. Grass is limited to the two small areas by Unit #10.
- All significant trees which have to be removed are replaced with suitable tree species and numbers.

Building Design

A three storey massing is used in each building cluster to reduce the building footprints; thereby reducing the impact on the site. The units within each cluster are stepped down the grade to reduce the cut and the overall mass of each cluster. Gables and bays are used to articulate the individual units. Exterior cladding is organized to emphasize vertical articulation and provide a residential feel.

The subject property has a limited width of 48.8m, and as a result of this narrowness, vehicle access has pushed the building clusters toward the property edges. The siting limits the ground level open space for each unit. The open space for each cluster is limited to a rear open deck for each of the units. The south facing decks for Building Clusters 2 & 3 are 1.8m from the south property line. A planted, fast growing western red cedar hedge runs the length of this property line which provides privacy from the abutting neighbours. The abutting neighbours have rear year yards with considerable depth.

Required Variances

▪ *Building Height:*

The maximum allowable building height is 9m. The table below illustrates the height variances required for each of the clusters.

Building Cluster	Maximum Allowable Building Height (m)	Proposed Building Height (m)	Variance Requested (m)
Building 1 (Units 1, 2 & 3)	9	10.15	1.15
Building 2 (Units 4, 5 & 6)	9	9.70	.70
Building 3 (Units 7, 8 & 9)	9	10.45	1.45

▪ *Minimum Landscape Treatment Level 2d*

The Minimum Landscape Treatment Level 2d is required along the north property line of 3219 Hammond Bay Road, and consists of a 1.8m planting width, trees and a 1.8m screening fence. The Minimum Landscape Treatment Level 2d is varied to a 0.2m planting width with a 1.8m green wall fence and no trees.

DAP Recommendation

At its meeting held on 2011-DEC-08, the Design Advisory Panel accepted DP000755 as presented, with support for the variances and made the following recommendations:

- *be aware of the sight lines/views between the buildings and consider using columnar trees to provide screening;*
- *consider incorporating a railing between the driveway and the stairs;*
- *consider incorporating textual changes in the paving surface in order to provide better pedestrian flow through the site;*
- *consider how vehicles will maneuver through the site and possibly incorporating a turnaround area at the top of the driveway; and*
- *consider how the rainwater runoff will be managed.*

The applicant has reviewed the recommendations and has made the following changes:

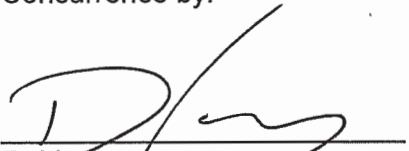
- columnar trees are used for screening and improved privacy;
- a concrete surface and concrete pavers are installed in unit parking aprons and visitor parking spaces;
- a gravel insert is sited at the top of the drive to facilitate vehicle turning; and
- a bioswale system is designed along the edges of the site. The bioswale along the east side of Building 1 is designed to maintain the hydrology for the existing trees at 3219 Hammond Bay Road.

Respectfully submitted,



B. Anderson, MCIP, RPP
MANAGER
PLANNING & DESIGN SECTION

Concurrence by:



D. Lindsay
DIRECTOR, MCIP, RPP
COMMUNITY DEVELOPMENT



I. Howat
GENERAL MANAGER
CORPORATE SERVICES

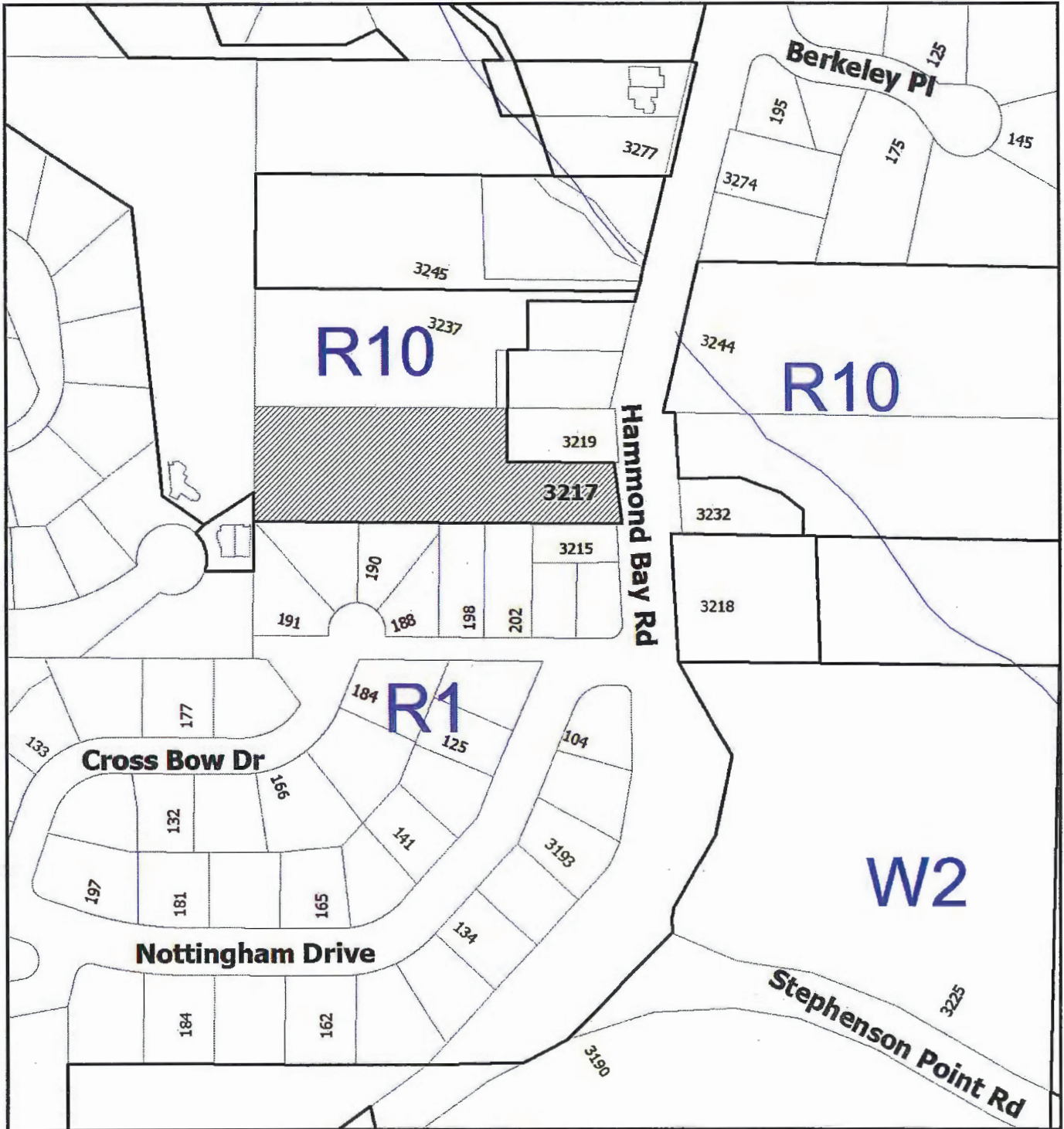
CITY MANAGER COMMENT:

I concur with the staff recommendation.

*Drafted: 2013-NOV-21
Prospero attachment: DP000755
GN/lb*

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SCHEDULE A



DEVELOPMENT PERMIT NO. DP000755

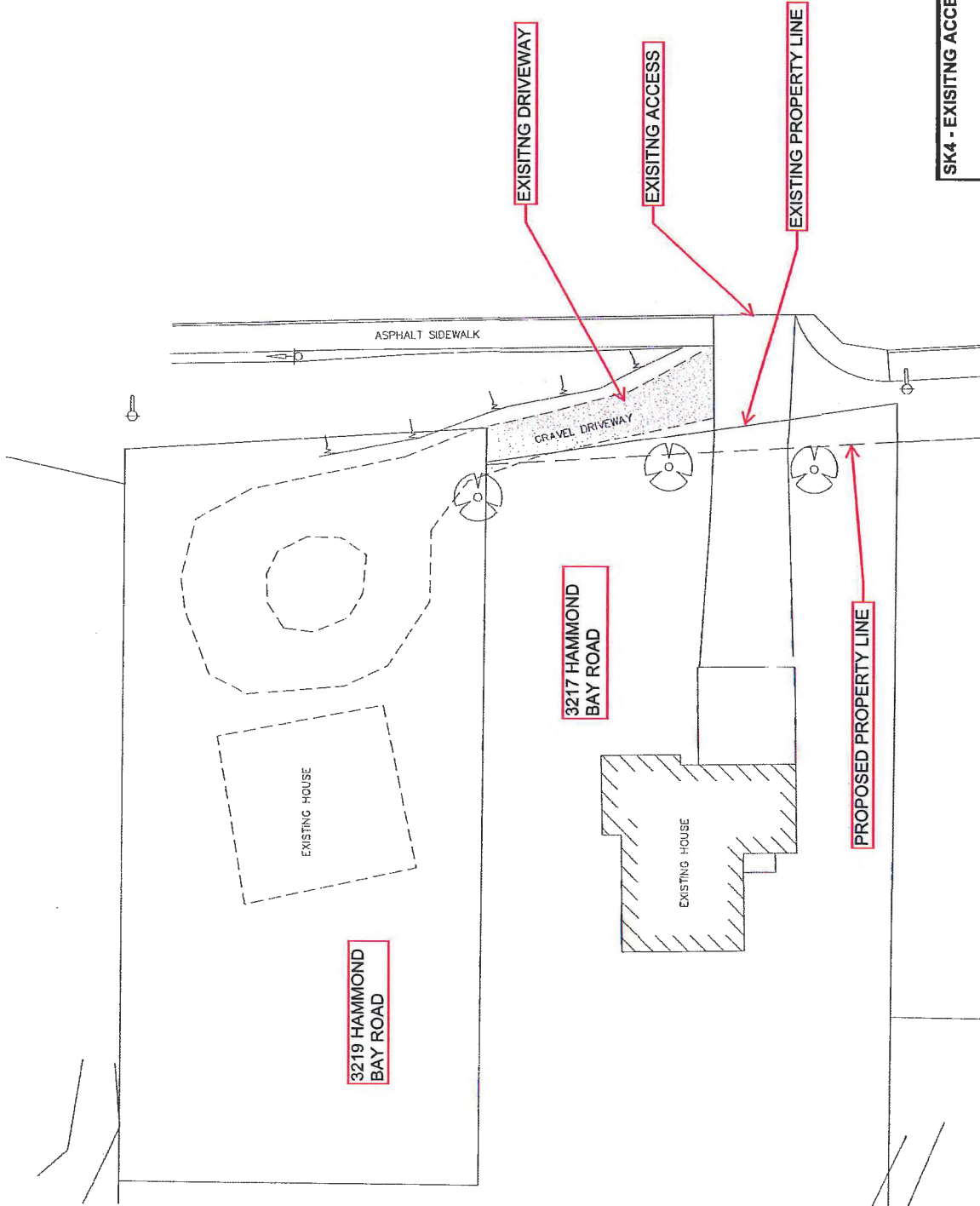
LOCATION PLAN

Civic: 3217 Hammond Bay Road
Lot 4, Section 15-A, Wellington District,
Plan 8519, Except that Part in Plan 19110



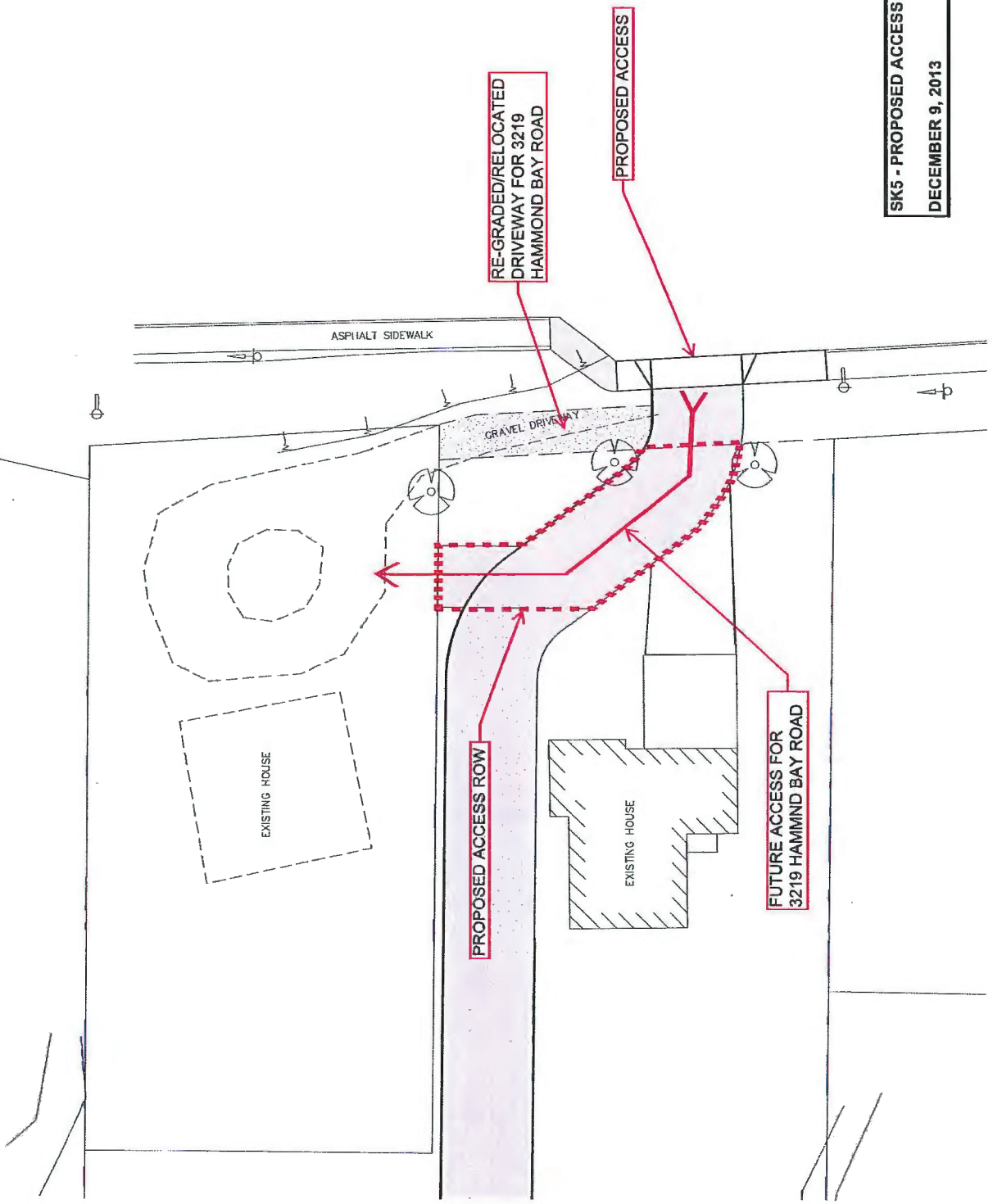
 **Subject Property**

Development Permit No. DP000755
3217 Hammond Bay Road
Schedule I
Existing Access Condition
to 3219 Hammond Bay Road



SK4 - EXISTING ACCESS
DECEMBER 9, 2013

Development Permit No. DP000755
3217 Hammond Bay Road
Schedule J
Proposed Access to
3219 Hammond Bay Road &
Interim Works and Services



SK5 - PROPOSED ACCESS
DECEMBER 9, 2013